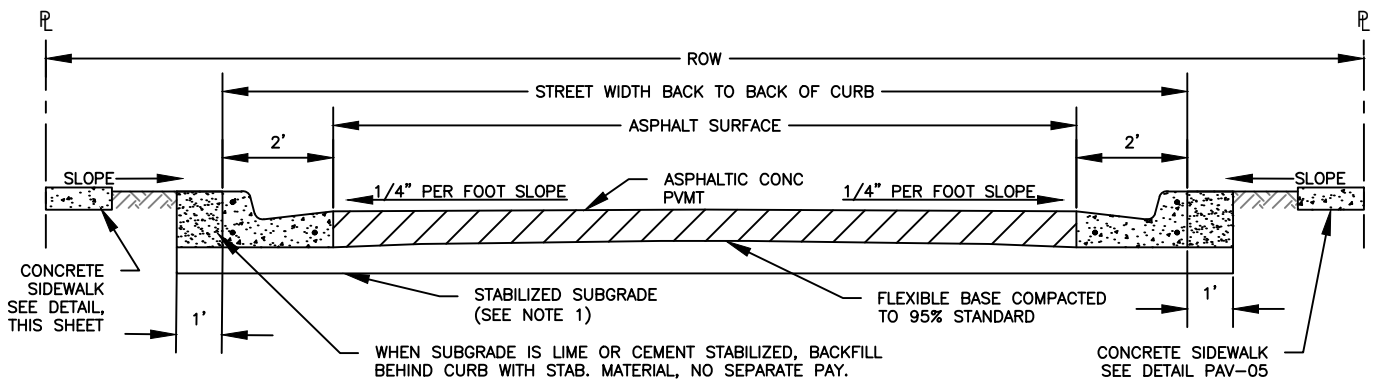
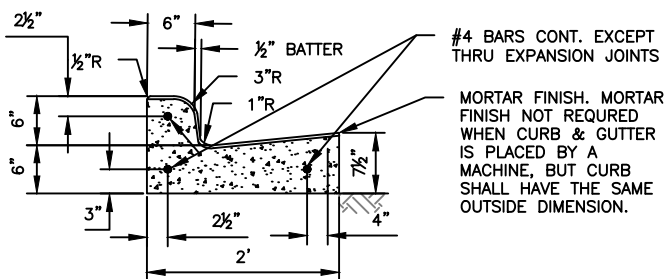


**TYPICAL ASPHALT PAVEMENT SECTION
WITH NO CURB & GUTTER**
NTS



**TYPICAL ASPHALT PAVEMENT SECTION
WITH CURB & GUTTER**
NTS



NOTE:

1. EXPANSION JOINTS ARE TO BE CONSTRUCTED AT CURB RETURNS, DRIVEWAY APPROACH SLABS, AND AT 60' MAXIMUM INTERVALS. 3 EACH - 5/8"x10" DOWEL PINS WITH SLEEVES TO BE USED AT EXPANSION JOINTS.

**CURB AND GUTTER DETAIL FOR
ASPHALT PAVEMENT**
NTS

NOTES:

1. STREET STRUCTURAL SECTION SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF TEXAS FOR ALL STREET TYPES.
2. STREET STRUCTURAL SECTION FOR LOCAL RESIDENTIAL STREETS SHALL BE DESIGNED AS DESCRIBED IN NOTE 1, BUT SHALL BE NO LESS THAN: 1 1/2-INCH THICK HOT MIX ASPHALTIC CONCRETE, 6-INCH THICK FLEXIBLE BASE, AND 6-INCH THICK STABILIZED SUBGRADE FOR GRANULAR SOILS (OR 8-INCH THICK STABILIZED SUBGRADE FOR COHESIVE SOILS).
3. FLEXIBLE BASE AND STABILIZED SUBGRADE SHALL BE COMPACTED TO 95% STANDARD PROCTOR (ASTM D-698), AT A MOISTURE CONTENT OF OPTIMUM TO 3 PERCENT ABOVE OPTIMUM.
4. COHESIVE SOILS SHALL BE STABILIZED WITH LIME OR LIME-FLYASH, AND GRANULAR SOILS SHALL BE STABILIZED WITH CEMENT. LIME, LIME-FLYASH OR CEMENT PERCENT APPLICATION AND DEPTH OF STABILIZATION SHALL BE AS DETERMINED BY A GEOTECHNICAL ENGINEER REGISTERED IN THE STATE OF TEXAS. COHESIVE SOILS ARE DEFINED AS SOILS WITH 50% OR MORE (BY WEIGHT) PASSING A No 200 SIEVE AND PLASTICITY INDEX GREATER THAN SEVEN. GRANULAR SOILS ARE DEFINED AS SOILS WITH 50% OR MORE (BY WEIGHT) RETAINED ON No 200 SIEVE.



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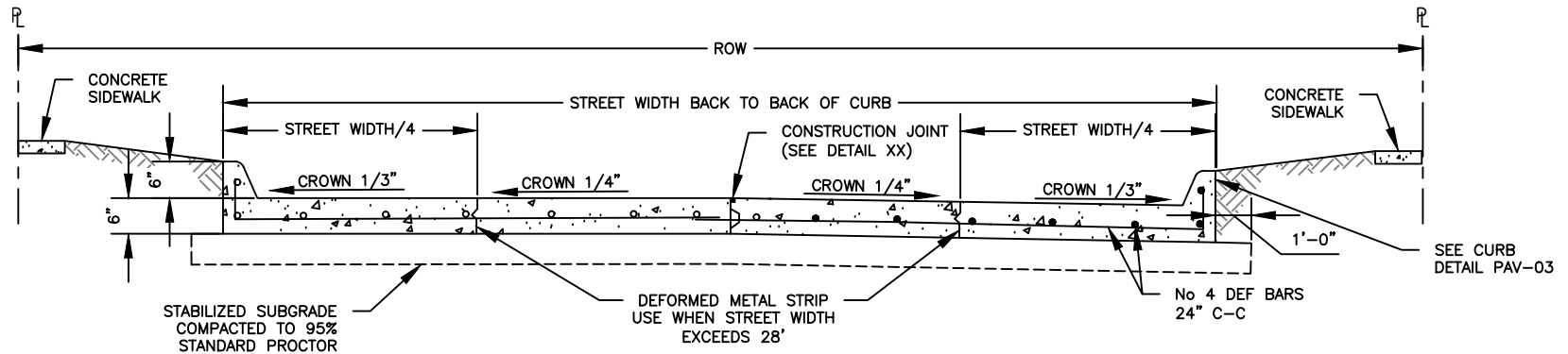
**ASPHALT PAVEMENT DETAILS WITH
AND WITHOUT CURB AND GUTTERS**

CITY OF MAGNOLIA STANDARD DETAIL

DETAIL NO.

PAV-01

EFFECTIVE DATE: 8/3/2022



NOTES:

1. STREET STRUCTURAL SECTION SHALL BE DESIGNED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF TEXAS FOR ALL STREET TYPES.
2. STREET STRUCTURAL SECTION FOR LOCAL RESIDENTIAL STREETS SHALL BE DESIGNED AS DESCRIBED IN NOTE 1, BUT SHALL BE NO LESS THAN: 6-INCH THICK PORTLAND CEMENT CONCRETE AND 6-INCH THICK STABILIZED SUBGRADE FOR GRANULAR SOILS (OR 8-INCH THICK STABILIZED SUBGRADE FOR COHESIVE SOILS).
3. STABILIZED SUBGRADE COMPACTED TO 95% STANDARD PROCTOR (ASTM D-698), AT A MOISTURE CONTENT OF OPTIMUM TO 3 PERCENT ABOVE OPTIMUM.
4. COHESIVE SOILS SHALL BE STABILIZED WITH LIME OR LIME-FLYASH, AND GRANULAR SOILS SHALL BE STABILIZED WITH CEMENT. LIME, LIME-FLYASH OR CEMENT PERCENT APPLICATION AND DEPTH OF STABILIZATION SHALL BE AS DETERMINED BY A GEOTECHNICAL ENGINEER REGISTERED IN THE STATE OF TEXAS. COHESIVE SOILS ARE DEFINED AS SOILS WITH 50% OR MORE (BY WEIGHT) PASSING A No 200 SIEVE AND PLASTICITY INDEX GREATER THAN SEVEN. GRANULAR SOILS ARE DEFINED AS SOILS WITH 50% OR MORE (BY WEIGHT) RETAINED ON No 200 SIEVE.



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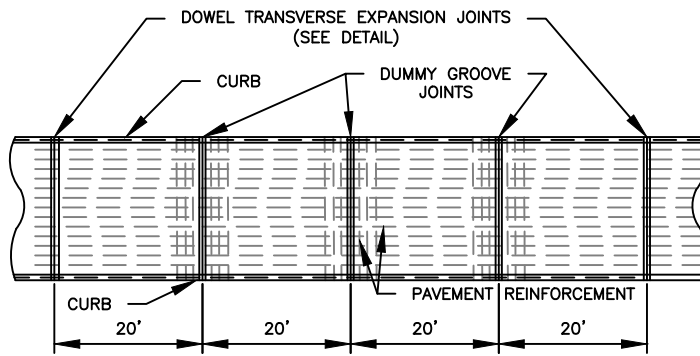
**TYPICAL CONCRETE
PAVEMENT SECTION**

CITY OF MAGNOLIA STANDARD DETAIL

DETAIL NO.

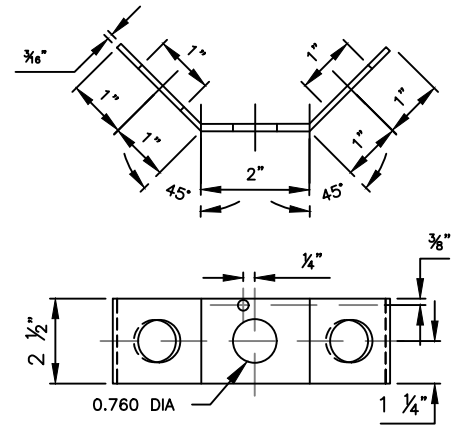
PAV-02

EFFECTIVE DATE: 8/3/2022



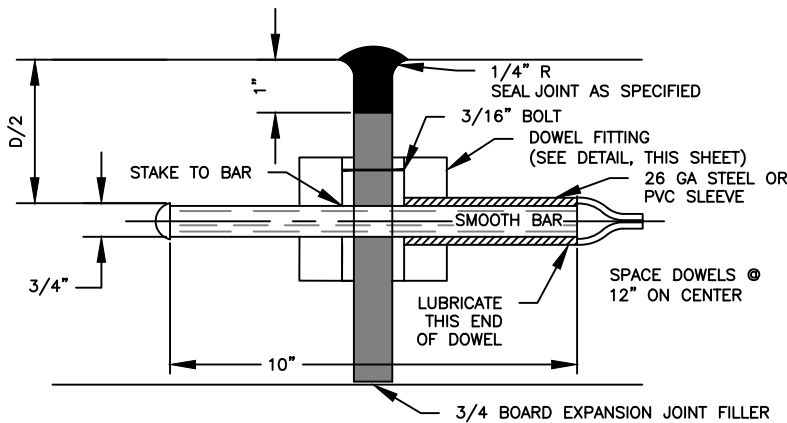
EXPANSION AND CONTRACTION JOINT LOCATIONS
ON CONCRETE PAVEMENTS

NTS



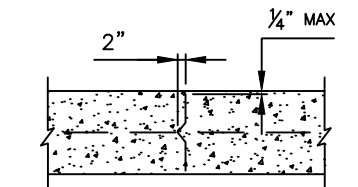
DOWEL FITTING

NTS



DOWEL TYPE EXPANSION JOINT
IN CONCRETE PAVEMENT

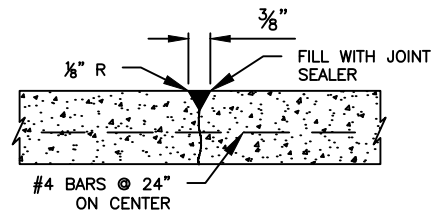
NTS



DEFORMED METAL STRIP
CONTRACTION JOINT

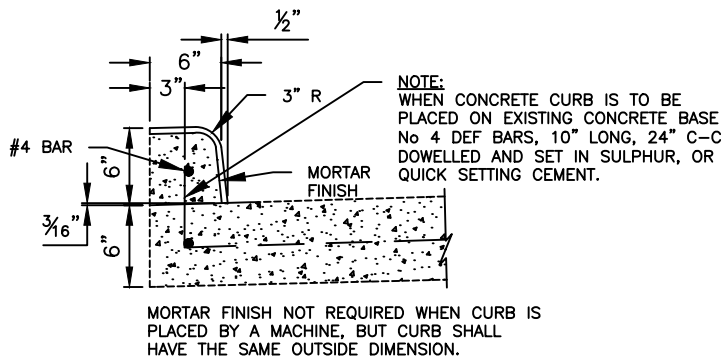
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NOTE:
CONTRACTION GROOVE DEPTH SHALL BE A MIN OF $\frac{1}{4}$ OF THICKNESS OF CONCRETE.



DUMMY GROOVE
CONTRACTION JOINT

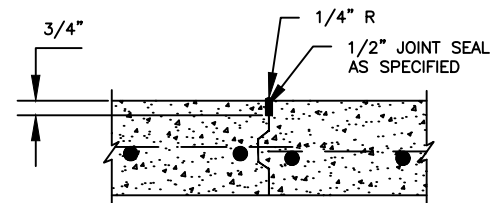
NTS



MORTAR FINISH NOT REQUIRED WHEN CURB IS PLACED BY A MACHINE, BUT CURB SHALL HAVE THE SAME OUTSIDE DIMENSION.

CONCRETE CURB

NTS



CONSTRUCTION JOINT — KEYED

NTS



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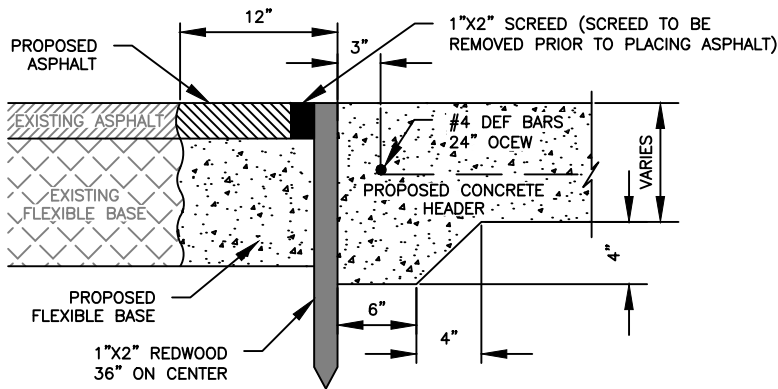
DOWEL AND JOINT FITTING DETAILS

CITY OF MAGNOLIA STANDARD DETAIL

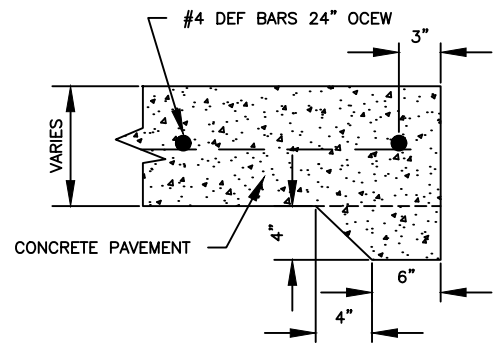
DETAIL NO.

PAV-03

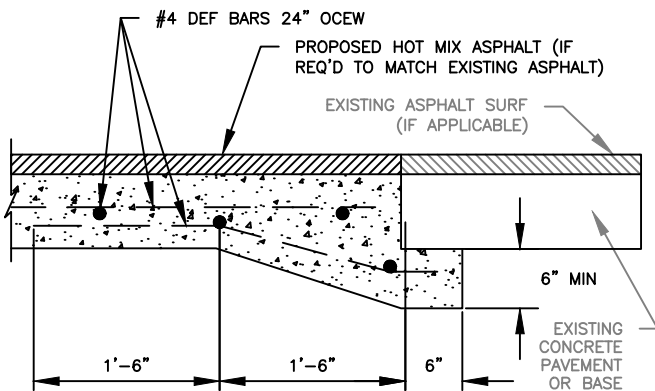
EFFECTIVE DATE: 8/3/2022



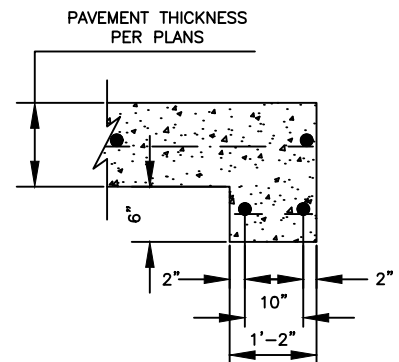
PAVING HEADER
(FOR USE IN CONNECTING CONCRETE
PAVEMENT TO ASPHALT PAVEMENT)
NTS



**STANDARD CONCRETE
PAVEMENT HEADER**
(FOR USE AT END OF ANY CONCRETE
PAVEMENT WHICH DOES NOT TIE TO
CONCRETE OR ASPHALT PAVEMENT)
NTS



UNDERCUT DETAIL
(USE FOR NEW CONCRETE PAVEMENT
TIE TO EXISTING CONCRETE PAVEMENT)
NTS



**CONCRETE PAVEMENT
RAILROAD HEADER**
NTS



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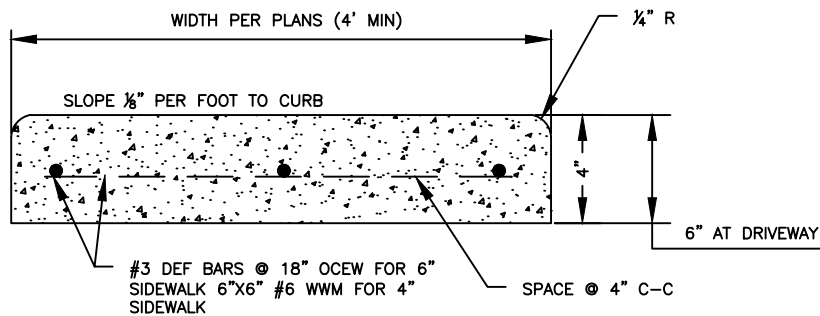
PAVEMENT HEADER DETAILS

CITY OF MAGNOLIA STANDARD DETAIL

DETAIL NO.

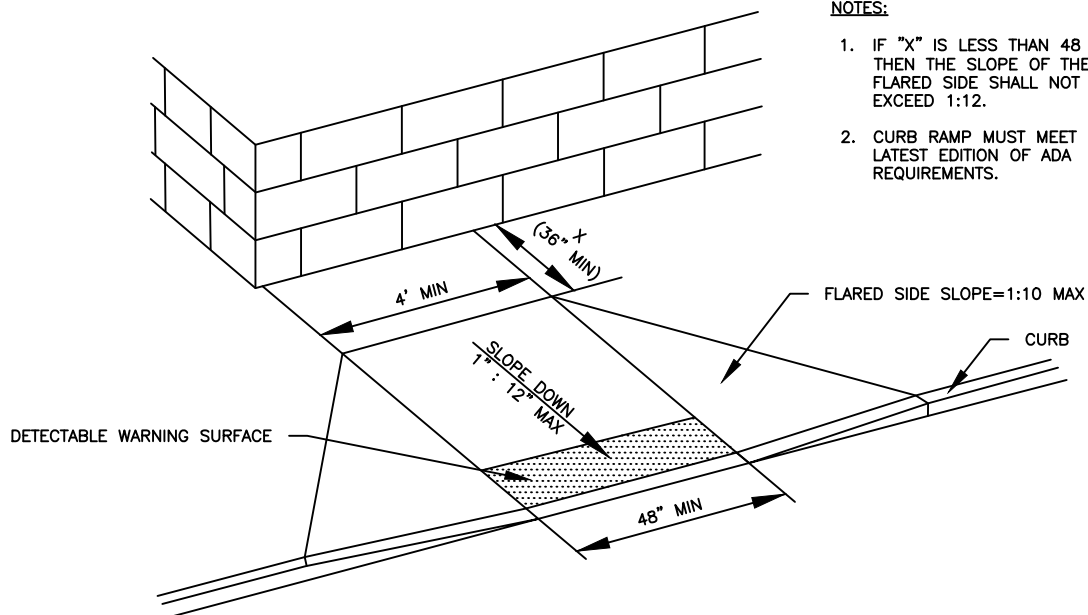
PAV-04

EFFECTIVE DATE: 8/3/2022



DUMMY GROOVE CONTRACTION JOINTS TO BE 1" DEEP @ 5' SPACING. EXPANSION JOINTS TO BE 1/2" BITUMINIOUS JOINT FILLER @ 20' SPACING. SEAL EXPANSION JOINT WITH 1/2" THICK JOINT SEALER. FINISHED GRADE ADJACENT TO SIDEWALK TO BE 0.1' BELOW CONCRETE.

CONCRETE SIDEWALK
NTS



NOTES:

1. IF "X" IS LESS THAN 48 IN, THEN THE SLOPE OF THE FLARED SIDE SHALL NOT EXCEED 1:12.
2. CURB RAMP MUST MEET LATEST EDITION OF ADA REQUIREMENTS.

CURB RAMP
NTS



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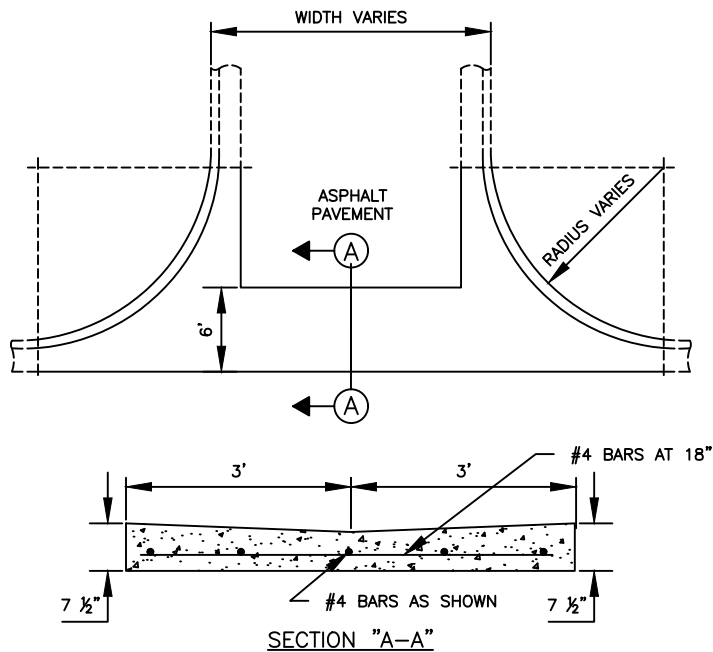
**CONCRETE SIDEWALK AND
CURB RAMP DETAILS**

CITY OF MAGNOLIA STANDARD DETAIL

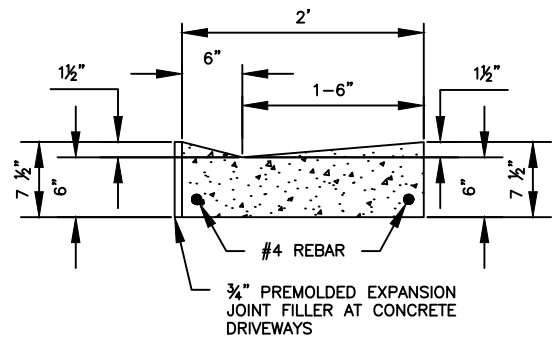
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PAV-05

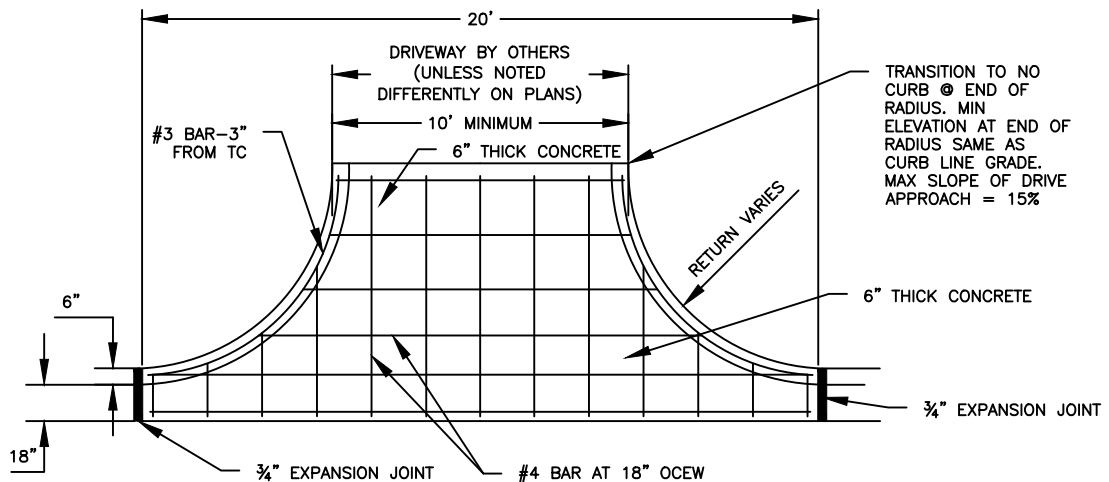
EFFECTIVE DATE: 8/3/2022



TYPICAL VALLEY GUTTER
NTS



GUTTER AT DRIVE
(WHEN DRIVEWAY LOCATIONS ARE KNOWN
WHEN CURB AND GUTTER IS INSTALLED)
NTS



NOTES:

1. MATCH TOP OF CURB AND SIDEWALK. TOP OF CURB AND DRIVE APRON WILL COME TOGETHER AT THE END OF THE CURB RETURN.
2. FOR NEW DRIVEWAY ON EXISTING STREET, SAWCUT AND COMPLETELY REMOVE EXISTING CURB AND GUTTER SECTION. TIE INTO CURB AND GUTTER REINFORCING WITH NEW DRIVEWAY REINFORCING. SEAL CRACK WHERE ASPHALT MEETS NEW CONCRETE WITH JOINT SEALER.
3. FOR EXISTING DRIVEWAY, REPLACE TO NEXT EXPANSION JOINT.

STANDARD DRIVE APPROACH
NTS



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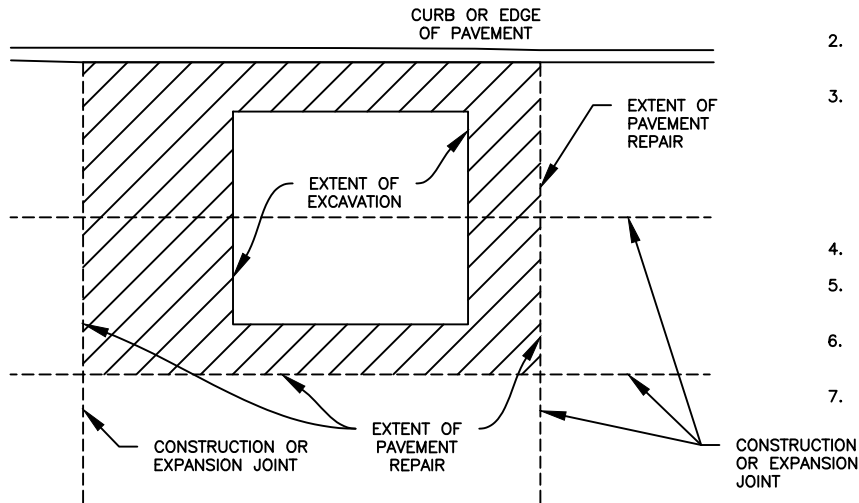
STANDARD DRIVE AND
GUTTER DETAILS

CITY OF MAGNOLIA STANDARD DETAIL

DETAIL NO.

PAV-06

EFFECTIVE DATE: 8/3/2022

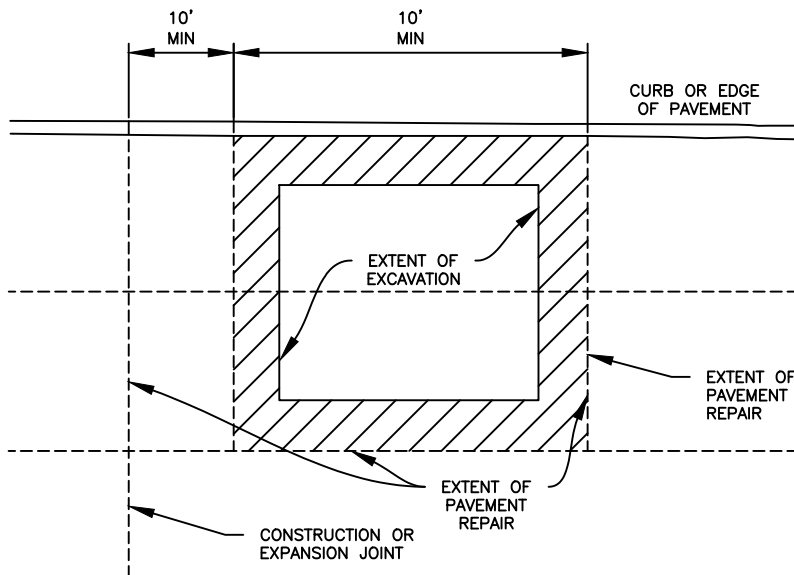


NOTES:

1. EXTENT OF PAVEMENT REPAIR SHALL BE PERPENDICULAR AND PARALLEL TO TRAVEL WAY.
2. REPLACE ENTIRE PANEL WIDTH AND LENGTH TO NEAREST CONSTRUCTION OR EXPANSION JOINT BEYOND EDGE OF EXCAVATION.
3. SAW CUT AND EXPOSE 15" OF REINFORCING STEEL WITHIN EXISTING PAVEMENT. PROVIDE HORIZONTAL DOWELS IF EXISTING REINFORCING IS BROKEN OFF. HORIZONTAL DOWELS SHALL BE #6 BARS, 24" LONG, 24" C-C, DRILLED AND EMBEDDED 8" INTO THE CENTER OF THE EXISTING CONCRETE PAVEMENT.
4. REPLACE CURB WHEN ADJACENT LANE IS REPLACED.
5. MAINTAIN EXPANSION JOINTS AT EXISTING LOCATIONS UNLESS OTHERWISE DIRECTED BY CITY ENGINEER.
6. SPECIALTY PAVEMENTS (IE: BRICK PAVERS) TO BE REPLACED WITH MATCHING PAVEMENT IN ALL CASES.
7. REPLACE PAVEMENT MARKINGS.

**STREET CUT PAVEMENT REPLACEMENT – CONCRETE PAVEMENT
LESS THAN OR EQUAL TO 5 YEARS IN AGE**

NTS



NOTES:

1. EXTENT OF PAVEMENT REPAIR SHALL BE PERPENDICULAR AND PARALLEL TO RAVEL WAY.
2. WIDTH:
REPLACE PANEL WIDTH TO NEAREST CONSTRUCTION OR EXPANSION JOINT BEYOND EDGE OF EXCAVATION.
3. LENGTH:
A. MINIMUM LENGTH OF PAVEMENT REPAIR ALONG TRAVEL WAY IS 10' FROM THE NEAREST JOINT.
B. IF EDGE OF EXCAVATION IS LESS THAN 10' FROM EXISTING CONSTRUCTION OR EXPANSION JOINT, REPLACE PAVEMENT TO EXISTING JOINT.
4. SAW CUT AND EXPOSE 15" OF REINFORCING STEEL AROUND EDGE OF PANEL REPLACEMENT. PROVIDE HORIZONTAL DOWELS IF REINFORCING IS BROKEN OFF OR DOES NOT EXIST. HORIZONTAL DOWELS SHALL BE #6 BARS, 24" LONG, 24" C-C, DRILLED AND EMBEDDED 8" INTO THE CENTER OF THE EXISTING CONCRETE PAVEMENT.
5. REPLACE CURB WHEN ADJACENT LANE IS REPLACED.
6. MAINTAIN EXPANSION JOINTS AT EXISTING LOCATIONS UNLESS OTHERWISE DIRECTED BY CITY ENGINEER.
7. SPECIALTY PAVEMENTS (IE: BRICK PAVERS) TO BE REPLACED WITH MATCHING PAVEMENT IN ALL CASES.
8. REPLACE PAVEMENT MARKINGS.

**STREET CUT PAVEMENT REPLACEMENT – CONCRETE PAVEMENT
OVER 5 YEARS IN AGE**

NTS



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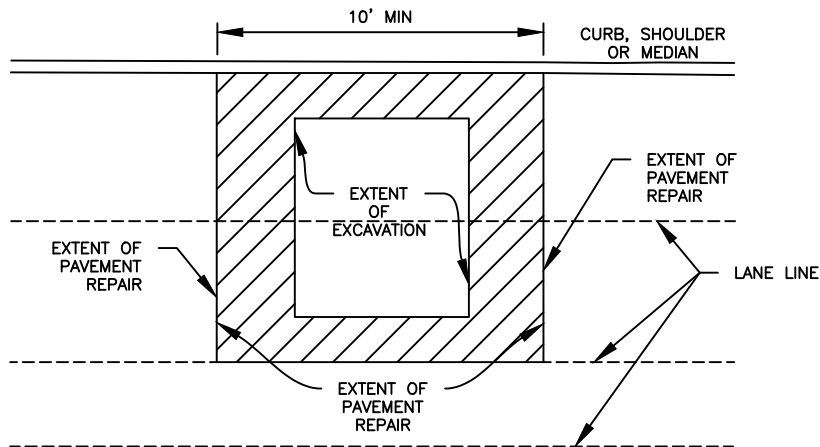
**STREET CUT CONCRETE PAVEMENT
REPLACEMENT DETAILS**

CITY OF MAGNOLIA STANDARD DETAIL

DETAIL NO.

PAV-07

EFFECTIVE DATE: 8/3/2022



**STREET CUT PAVEMENT REPLACEMENT – ASPHALT PAVEMENT
FOR PAVEMENT OF ALL AGES**
NTS

NOTES:

1. EXTENT OF PAVEMENT REPAIR SHALL BE PERPENDICULAR AND PARALLEL TO RAVEL WAY.
2. **FLEXIBLE BASE:**
REPLACE BASE TO SAME THICKNESS PLUS TWO INCHES (2") FOR EXTENT OF EXCAVATION. USE APPROVED BASE MATERIAL TYPE.
3. **SURFACE COURSE:**
 - A. **WIDTH:**
SURFACE MILL AND OVERLAY FULL WIDTH OF LANE(S) TO NEAREST LANE DIVIDER BEYOND EDGE OF EXCAVATION.
 - B. **LENGTH:**
MINIMUM LENGTH OF SURFACE MILL ALONG TRAVEL WAY IS 10'
 - C. REPLACE PAVEMENT MARKINGS.

**ADDITIONAL REQUIREMENTS FOR
ASPHALT OVERLAY ON CONCRETE
PAVEMENT:**

1. REPLACE CONCRETE FOR EXTENT OF EXCAVATION. REPLACE TO SAME THICKNESS PLUS TWO INCHES (2").
2. **WIDTH:**
IF EXCAVATION EXTENDS MORE THAN HALF OF A LANE, REPLACE ENTIRE LANE OF CONCRETE. OTHERWISE USE STANDARD DETAIL.
3. SAW CUT AND EXPOSE 15" OF REINFORCING STEEL AROUND EDGE OF CONCRETE REPLACEMENT. IF NO REINFORCING STEEL EXISTS, USE HORIZONTAL DOWELS. HORIZONTAL DOWELS SHALL BE #6 BARS, 24" LONG, 24" C-C, DRILLED AND EMBEDDED 8" INTO THE CENTER OF THE EXISTING CONCRETE PAVEMENT.
4. REPLACE CURB WHEN ADJACENT LANE IS REPLACED.
5. MAINTAIN CONCRETE EXPANSION JOINTS AT EXISTING LOCATIONS UNLESS OTHERWISE APPROVED BY CITY ENGINEER.



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**STREET CUT ASPHALT PAVEMENT
REPLACEMENT DETAILS**

CITY OF MAGNOLIA STANDARD DETAIL

DETAIL NO.

PAV-08

EFFECTIVE DATE: 8/3/2022